



National Committee on Uniform Traffic Control Devices

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Item No.: 25A-GMI-01

NCUTCD RECOMMENDATION FOR CHANGES TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES

COMMITTEE / TASK FORCE: Guide & Motorist Information Signs Technical Committee
ITEM NUMBER: 25A-GMI-01
TOPIC: Bus Lane Regulatory Signs
ORIGIN OF REQUEST: Transit MCTF and 20B-RW-02
AFFECTED SECTIONS OF MUTCD: 2G.03

After publication of the 2023
MUTCD, the NCUTCD rescinded all
MUTCD prior recommendations.
This proposed change is based on
one of those prior recommendations.

DEVELOPMENT HISTORY:

Approved by GMI TC: 01/09/2025, 06/11/2025
Approved by NCUTCD Council: 06/12/2025

This is a recommended change to the MUTCD that has been approved by the NCUTCD Council. This proposal does not represent a revision of the MUTCD and does not constitute official MUTCD standards, guidance, or options. It will be submitted to FHWA for consideration for inclusion in a future MUTCD revision. The MUTCD can be revised only through the federal rulemaking process.

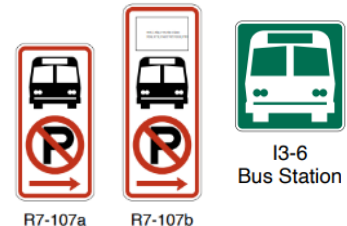
SUMMARY:

This proposal updates a previous NCUTCD recommendation (20B-RW-02) to include the proposed changes which allow bus symbol signs to be used on a preferential lane regulatory sign.

DISCUSSION:

Numerous metropolitan areas have implemented bus rapid transit (BRT)/busways in the past decade and more are planned. The MUTCD has numerous vehicular traffic warning signs. Symbols exist for bicycle, tractor, emergency vehicle, truck, golf cart, horse-drawn buggy and rail grade or trail crossings (see Section 2C.54). However, there is no comparable symbol use of bus symbols on preferential lane regulatory signs.

In January 2021 NCUTCD approved a comprehensive set of changes to the MUTCD related to Bus Transit/BRT Traffic Control (20B-RW-02). It included the bus symbols on preferential lane signs. Because approval of 20B-RW-02 was one month after the FHWA released the NPA for the MUTCD, the item was not forwarded to FHWA in time for the NPA. Time was not available in the NCUTCD docket comments to fully address these recommendations. The 2023 MUTCD has several bus lane changes but did not include the bus symbol for use in preferential lane signs. In January 2024, NCUTCD rescinded all recommended changes



pertaining to the 2009 MUTCD, so a new proposal is needed to address these items. This proposal updates the previous recommendation by specifically including the bus symbol, within the structure FHWA provided in the 2023 MUTCD. The proposal focuses solely on the changes in preferential lanes and does not attempt to fully address all the recommendations of 20B-RW-02. A recent proposed change (24A-RW-03) attempted to address this topic (including a warning sign) and was forwarded to sponsors in spring 2024 and was tabled by Council. For this reason, the aspects of 20B-RW-02 will be addressed by specific Technical Committees. This proposal only focuses on GMI elements of the prior proposals.

One change is proposed. The use of a bus symbol on preferential lane regulatory signs as recommended and approved in 20B-RW-02. The bus symbol already exists within the 2023 MUTCD and is used on the R7-107a, R7-107b and I3-6 signs. Based upon sponsor comments to the prior 2024 proposal that was tabled, eight signs were replaced with symbols (the existing “all” word signs may still be utilized per Section 2A.04 and additional text to specify such use). Three additional signs from 20B-RW-02 are also provided for overhead use. The representation of the dynamic message element hybrid signs shown in Figure 2G-1 have been corrected for clarity and consistency with other use of lane control signals in the MUTCD. This change is necessary as no symbol signs can be utilized as traffic control devices unless they are specified in the MUTCD.



RECOMMENDED MUTCD CHANGES:

The following present the proposed changes to the current MUTCD within the context of the current MUTCD language. Proposed additions to the MUTCD are shown in blue underline and proposed deletions from the MUTCD are shown in ~~red strikethrough~~. Changes previously approved by NCUTCD Council (but not yet adopted by FHWA) are shown in green double underline for additions and ~~green double strikethrough~~ for deletions. In some cases, background comments may be provided with the MUTCD text. These comments are indicated by [bracketed white text in shaded green].

CHAPTER 2G. PREFERENTIAL AND MANAGED LANE SIGNS

Section 2G.02 Sizes of Preferential and Managed Lane Signs

**Table 2G-1. Managed and Preferential Lanes Sign and Plaque Minimum Sizes
(Sheet 1 of 2)**

Sign or Plaque	Sign Designation	Section	Single Lane	Multi-Lane	Expressway	Freeway	Oversized
Preferential Lane Vehicle Occupancy Definition (post-mounted)	R3-11 series	2G.05	30 x 42	30 x 42	36 x 60	78 x 96	78 x 96
Preferential Lane Ahead or Ends – High-Occupancy Vehicles (post-mounted)	R3-12 series, 12a, 12b, 12c, 12d, 12e	2G.06 2G.07	30 x 42	30 x 42	36 x 60	48 x 84	48 x 84
Preferential Lane Ahead or Ends (post-mounted)	R3-12f, 12g	2G.06 2G.07	30 x 36	30 x 36	36 x 48	48 x 60	48 x 60
Preferential Lane Ends (post-mounted)	R3-12h	2G.07	36 x 48	36 x 48	48 x 66	60 x 84	60 x 84
Preferential HOV Lane Operation (overhead)	R3-14, 14a, 14aX	2G.05	72 x 60	72 x 60	96 x 72	144 x 108	144 x 108
Preferential HOV Lane Ahead (overhead)	R3-15, R3-15d	2G.06	66 x 36	66 x 36	84 x 48	102 x 60	102 x 60
Preferential HOV Lane Ends	R3-15b, 15c, 15e	2G.07	66 x 36	66 x 36	84 x 48	102 x 60	102 x 60
Preferential Lane Ahead or Ends (overhead)	R3-15d, 15e	2G.06 2G.07	42 x 36	42 x 36	54 x 48	72 x 60	72 x 60

Section 2G.03 Regulatory Signs of Preferential Lanes – General

Standard:

01 When a preferential lane is established, the Preferential Lane regulatory signs (see Figure 2G-1) and pavement markings (see Chapter 3E) for these lanes shall be used to advise road users.

Support:

02 Preferential Lane (R3-10 series through R3-15 series) regulatory signs (see Figure 2G-1) consist of several different general types of regulatory signs as follows:

- A. Vehicle Occupancy Definition signs define the vehicle occupancy requirements applicable to an HOV lane (such as “2 OR MORE PERSONS PER VEHICLE”) or types of vehicles not meeting the minimum occupancy requirement (such as motorcycles or Inherently Low Emission Vehicles (ILEVs)) that are allowed to use an HOV lane (see Section 2G.04).
- B. Preferential Lane Operation signs notify road users of the days and hours during which the preferential restrictions are in effect (see Section 2G.05).
- C. Preferential Lane Advance signs notify road users that a preferential lane restriction begins ahead (see Section 2G.06).
- D. Preferential Lane Ends signs notify users of the termination point of the preferential lane restrictions (see Section 2G.07).

Standard

03 Regulatory signs applicable only to a preferential lane shall be distinguished from regulatory signs applicable to general-purpose lanes by the inclusion of the applicable symbol(s) and/or word(s) (see Figure 2G-1).

Support:

04 The symbol (such as the HOV or bus symbol) and word message displayed on a particular Preferential Lane regulatory sign will vary based on the specific type of allowed traffic and on other related operational constraints that have been established for a particular lane, such as an HOV lane, a bus lane, or a taxi lane.

Option:

05 Changeable message signs may supplement, substitute for, or be incorporated into static Preferential
Lane regulatory signs where travel conditions change or where multiple types of operational strategies
(such as variable occupancy requirements or vehicle types) are used and varied throughout the day or
week, or on a real-time basis, to manage the use of, control of, or access to preferential lanes.

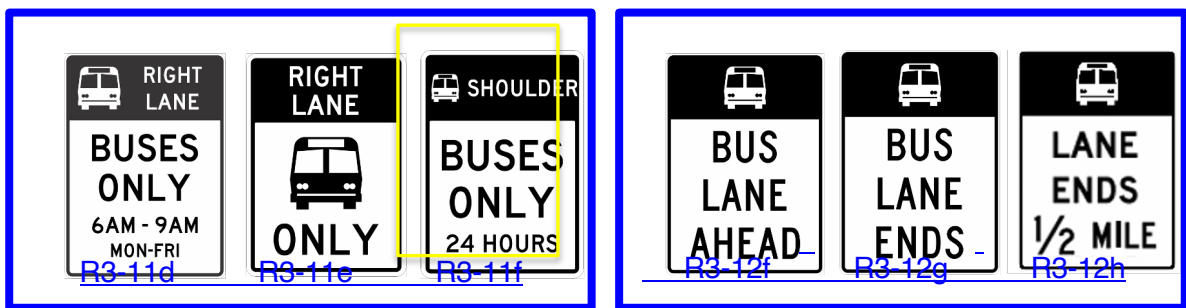
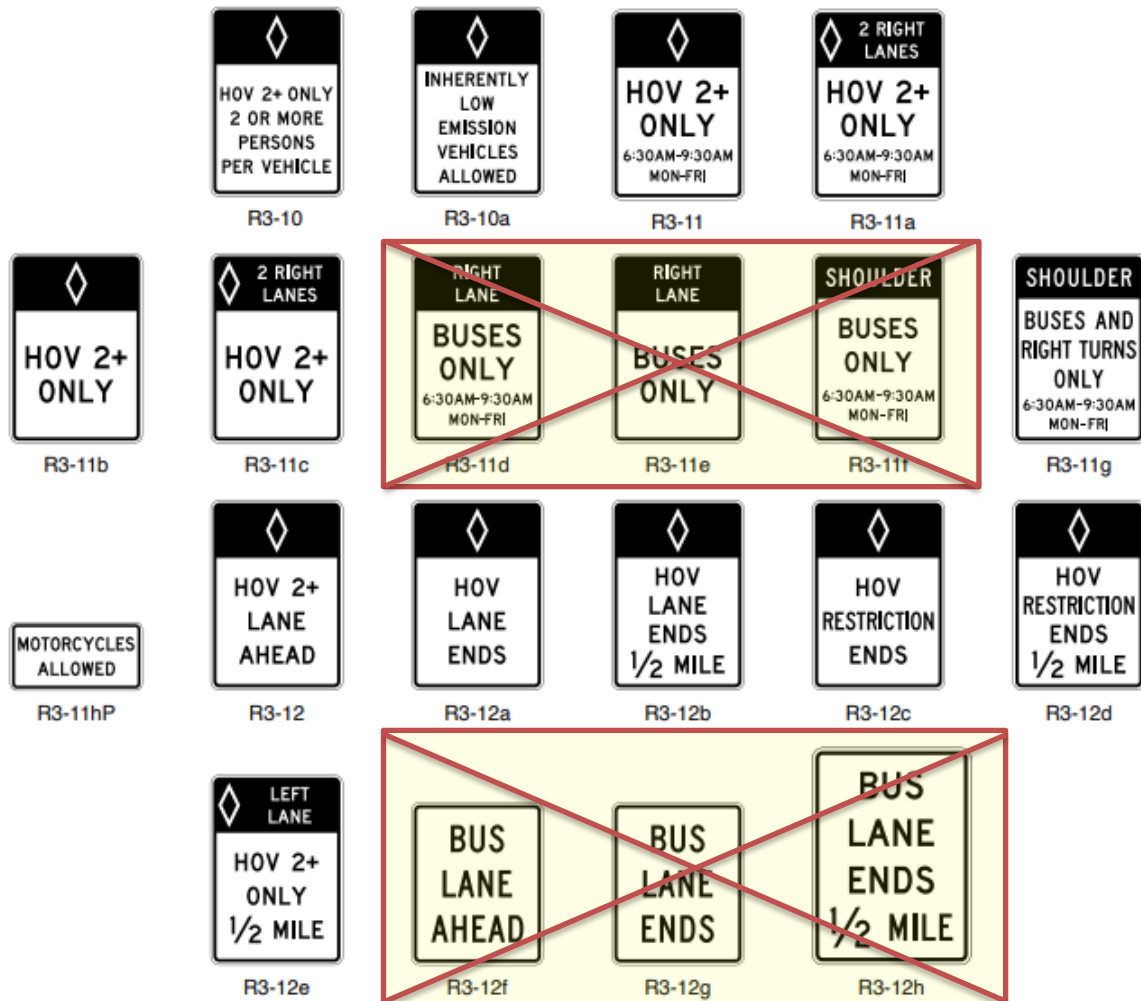
Support:

06 Figure 2G-1 illustrates examples of changeable messages incorporated into static Preferential Lane
regulatory signs displaying open and closed status using lane-use control signal indications (see Chapter
4T).

[All other paragraphs (P07-22) within 2G.03 remain the same].

Figure 2G-1. Preferential Lane Regulatory Signs and Plaque (Sheet 1 of 2)

A – Post-mounted Preferential Lane regulatory signs and plaque

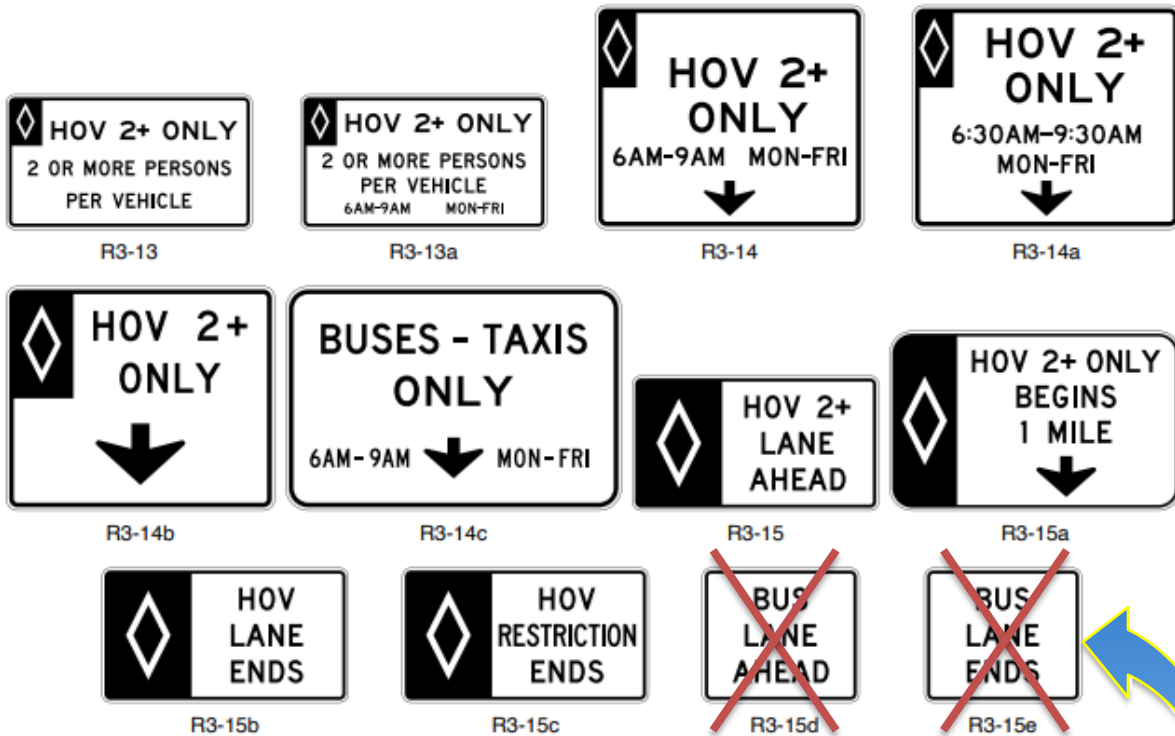


Notes:

1. The minimum vehicle occupancy requirement may vary for each facility (such as 2+, 3+, 4+).
2. The occupancy requirement may be added to the first line of the R3-12a, R3-12b, R3-12c, and R3-12d signs.
3. Some of the legends shown on these signs are for example purposes only. The specific legend for a particular application should be based upon local conditions, ordinances, and State statutes.

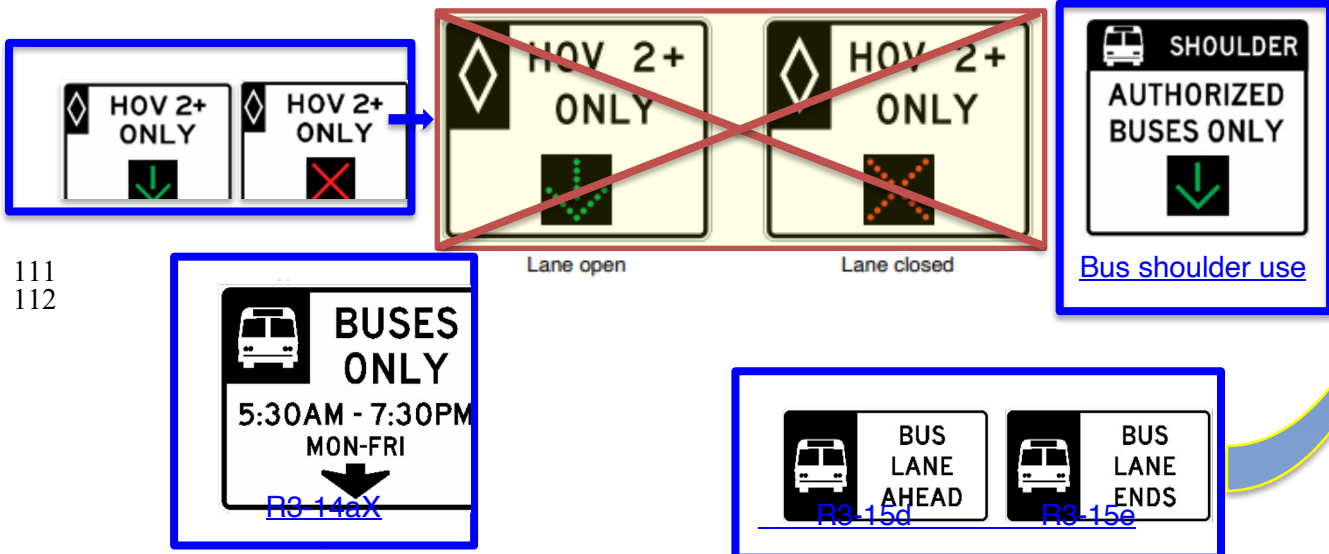
Figure 2G-1. Preferential Lane Regulatory Signs and Plaque (Sheet 2 of 2)

B – Overhead Preferential Lane regulatory signs



(see Section 2B.34) or shoulder bus (see Section 2G.20)

A lane-use control signal may be incorporated into an overhead preferential lane regulatory sign to indicate the status of a reversible operation as shown in the following examples:



Notes:

1. The minimum vehicle occupancy requirement may vary for each facility (such as 2+, 3+, 4+).
2. The occupancy requirement may be added to the first line of the R3-15b and R3-15c signs.
3. Some of the legends shown on these signs are for example purposes only. The specific legend for a particular application should be based upon local conditions, ordinances, and State statutes.
4. Where sufficient median width is available, the R3-13 series and R3-15 series signs may be post-mounted.