



National Committee on Uniform Traffic Control Devices

13236 North 7th Street, Suite 4-259, Phoenix, Arizona 85022
Phone/Text: 231-4-NCUTCD (231-462-8823)
E-mail: secretary@ncutcd.org Website: <https://ncutcd.org>

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National Committee on Uniform Traffic Control Devices (NCUTCD) Recommended Changes to Proposed Text for 11th Edition of the MUTCD Docket Number: FHWA-2020-0001

6 **Federal Register Item Number:** 375-381

7 **NPA MUTCD Section Number:** Sections 3J.01-3J.07

8 **Legend:** Base text shown in proposal is the NPA “clean” proposed text.

- 9 • [NCUTCD recommendation for text to be added in final rule.](#)
- 10 • ~~NCUTCD recommendation for text to be deleted in final rule.~~
- 11 • [NCUTCD recommendation for text to be moved/relocated in final rule.](#)
- 12 • NPA text that was not previously approved by NCUTCD but is now approved.
- 13 • Explanatory note: [\[Note that explains purpose of recommended change.\]](#)

15 The following pages present NCUTCD recommendations for changes to the MUTCD NPA
16 proposed text, tables, and figures for Chapter 3J. Below is a short summary of the NCUTCD
17 position for each section of this chapter. A more detailed summary is provided at the beginning
18 of each section.

- 19
- 20 • NPA #375, Section 3J.01: NCUTCD agrees with NPA content (no changes recommended).
 - 21 • NPA #376, Section 3J.02: NCUTCD agrees with NPA content (no changes recommended).
 - 22 • NPA #377, Section 3J.03: NCUTCD agrees with NPA content (no changes recommended).
 - 23 • NPA #378, Section 3J.04: NCUTCD agrees with NPA content (no changes recommended).
 - 24 • NPA #379, Section 3J.05: NCUTCD agrees with NPA content (no changes recommended).
 - 25 • NPA #380, Section 3J.06: NCUTCD agrees with NPA content (no changes recommended).
 - 26 • NPA #381, Section 3J.07: NCUTCD agrees with NPA content (no changes recommended).
- 27

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30 **CHAPTER 3J. MARKING AND DELINEATION OF ISLANDS AND CURB**
31 **EXTENSIONS**
32

33 **Section 3J.01 Comments: NCUTCD agrees with 3J.01 as presented in the NPA.**
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35 **Section 3J.01 General**

36 Support:

37 This Chapter addresses the marking and delineation of islands (see definition in Section
38 1C.02) and also curb extensions designated by pavement markings. Definitions, types, sizes, and
39 other criteria for the design of islands are set forth in “A Policy on Geometric Design of
40 Highways and Streets” (see Section 1A.05).

41 Section 3C.12 contains information on pedestrian islands and medians.

42 Sections 3H.04 and 3H.05 contain information on colored pavement that can be used within
43 islands.

44 Option:

45 An island may be designated by curbs, pavement edges, pavement markings, channelizing
46 devices, or other devices.
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49 **Section 3J.02 Comments: NCUTCD agrees with 3J.02 as presented in the NPA.**
50

51 **Section 3J.02 Approach-End Treatment**

52 Support:

53 An approach-end treatment to an island consists of longitudinal pavement markings and/or
54 channelizing devices upstream of the island followed by a divergence of those pavement
55 markings and/or channelizing devices concluding with a transition to other pavement markings
56 that demarcate or outline the island (see Figure 3J-1).

57 Section 3B.13 contains information on pavement marking that function as approach-end
58 treatments for obstructions.

59 *Guidance:*

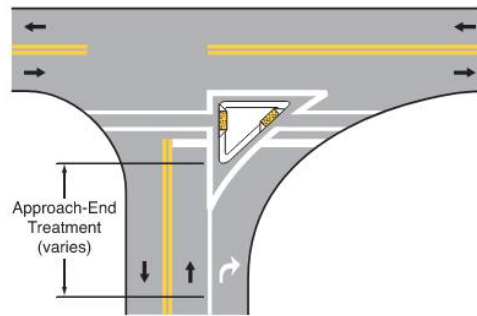
60 *The ends of islands first approached by traffic should be provided an approach-end*
61 *treatment, or curb markings (see Section 3J.04), or both to guide vehicles into desired paths of*
62 *travel along the island edge.*

63 *When raised bars or buttons that project more than 1 inch above the pavement surface are*
64 *used to create a rumble section in the neutral area, the raised bars or buttons should be marked*
65 *with white or yellow retroreflective materials, as determined by the direction or directions of*
66 *travel they separate.*
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69 **Figure 3J-1 Comments: NCUTCD agrees with Figure 3J-1 as presented in the NPA.**

70 **Figure 3J-1. Example of Markings for Approach End-Treatment to an Island**

Figure 3J-1. Example of an Approach-End Treatment for an Island



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73 **Section 3J.03 Comments: NCUTCD agrees with 3J.03 as presented in the NPA.**

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75 Section 3J.03 Islands Designated by Pavement Markings

76 Standard:

77 Except as provided in Paragraph 2, islands formed by pavement markings only shall be
78 established using channelizing lines, and shall be white when separating traffic flows in the
79 same general direction or yellow when separating opposing directions of traffic.

80 If a continuous flush median island separating travel in opposite directions is used, two
81 sets of solid double yellow lines shall be used to form the island (see Figure 3B-5). Other
82 markings in the median island area, such as diagonal lines (see Section 3B.25), shall also be
83 yellow, except crosswalk markings which shall be white (see Chapter 3C).

84 If used, chevrons and diagonal markings (see Section 3B.25) within the island shall be
85 the same color as the channelizing line.

86 Option:

87 Both chevrons and diagonal markings of the same color may be used within the same island
88 based on engineering judgment.

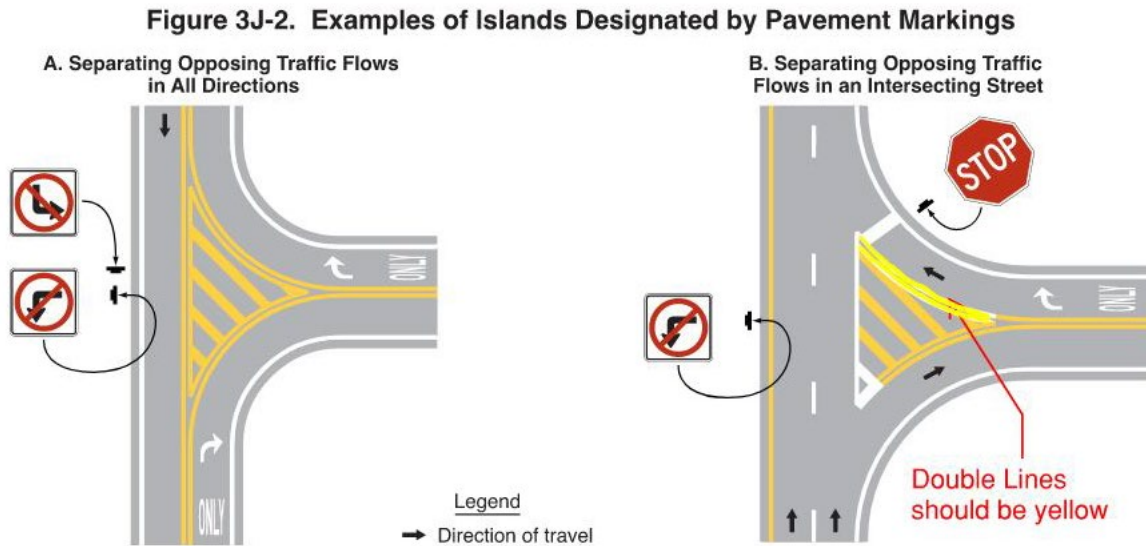
89 Support:

90 Figure 3J-2 illustrates examples of islands designated by pavement markings.

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92 **Figure 3J-2 Comments:** NCUTCD generally agrees with Figure 3J-2 as presented in the NPA,
93 but recommends changing the left edge lines in Figure 3J-2B to double yellow lines.

94 **Figure 3J-2. Examples of Islands Designated by Pavement Markings**



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Section 3J.04 Comments: NCUTCD agrees with 3J.04 as presented in the NPA.

100 Section 3J.04 Curb Markings for Raised Islands

101 Standard:

102 **Where curbs are marked for delineation or visibility purposes, the colors shall comply**
103 **with the general principles of markings (see Section 3A.03).**

104 Guidance:

105 *Retroreflective solid yellow curb markings should be placed on the approach ends of raised*
106 *medians and curbs of islands that are located in the line of traffic flow where the curb serves to*
107 *channel traffic to the right of the obstruction (see Figure 3J-3).*

108 *Retroreflective solid white curb markings should be used when traffic is permitted to pass on*
109 *either side of the island (see Figure 3J-3).*

110 *The retroreflective area should be of sufficient length to denote the general alignment of the*
111 *edge of the island along which vehicles travel, including the approach end, when viewed from*
112 *the approach to the island.*

113 Option:

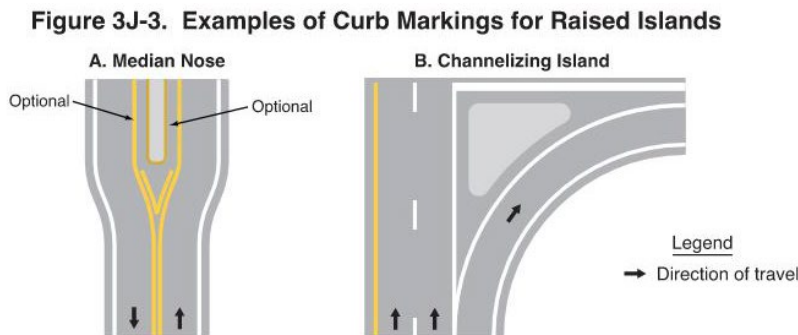
114 Where the curbs of the islands become parallel to the direction of traffic flow or where the
115 island is illuminated or marked with delineators, curb markings may be discontinued based on
116 engineering judgment or study.

117 Curb markings at openings in a continuous median island may be omitted based on
118 engineering judgment or study.

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120 **Figure 3J-3 Comments:** NCUTCD agrees with Figure 3J-3 as presented in the NPA.

121 **Figure 3J-3. Example of Curb Markings for Raised Islands**



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124 **Section 3J.05 Comments:** NCUTCD agrees with 3J.05 as presented in the NPA.

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126 **Section 3J.05 Pavement Markings for Raised Islands**

127 **Support:**

128 Pavement markings for raised islands include the approach-end treatment (see Section 3J.02),
129 channelizing lines, edge lines, and chevron or diagonal markings.

130 **Option:**

131 Solid yellow edge lines may be used adjacent to raised islands separating travel in opposite
132 directions (see Drawing A of Figure 3J-3).

133 **Standard:**

134 **Except as provided in Paragraph 4 and 6, raised islands separating traffic flows in the**
135 **same general direction shall be outlined with white channelizing lines (see Drawing A of**
136 **Figure 3J-4).**

137 **Option:**

138 Pavement markings for smaller raised islands may be omitted based on engineering
139 judgment.

140 **Guidance:**

141 Smaller raised islands that do not apply channelizing lines, edge lines, or chevron or
142 diagonal lines should be provided curb markings (see Section 3J.04).

143 Where traffic passes on the right of a raised island separating traffic flows in the same
144 general direction, a yellow edge line may be used adjacent to raised islands of discernible size
145 or length instead of continuing the white channelizing line from the approach-end treatment (see
146 Drawing B of Figure 3J-4).

147 **Support:**

148 Yellow edge lines adjacent to raised islands that separate traffic flows in the same general
149 direction can be advantageous as a countermeasure for wrong-way entry or travel if the yellow
150 edge line is of discernible length.

151 **Option:**

152 Chevron markings may be used in neutral areas formed by diverging channelizing lines at
153 raised islands separating traffic flows in the same general direction.

154 Diagonal markings of an appropriate color may be used in buffer areas between the
155 channelizing line and the raised island (see Figure 3J-5).

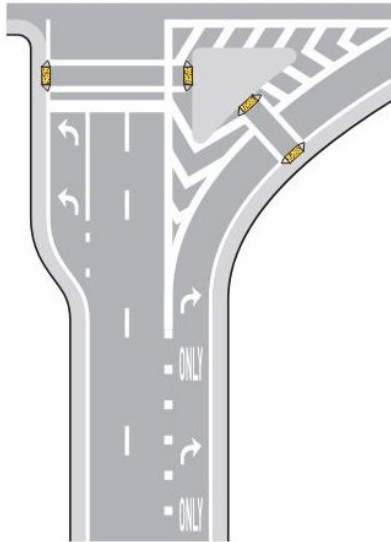
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157 **Figure 3J-4 Comments: NCUTCD agrees with Figure 3J-4 as presented in the NPA.**

158 **Figure 3J-4. Examples of Pavement Markings for Raised Islands (2 Sheets)**

Figure 3J-4. Example of Pavement Markings for Raised Islands (Sheet 1 of 2)

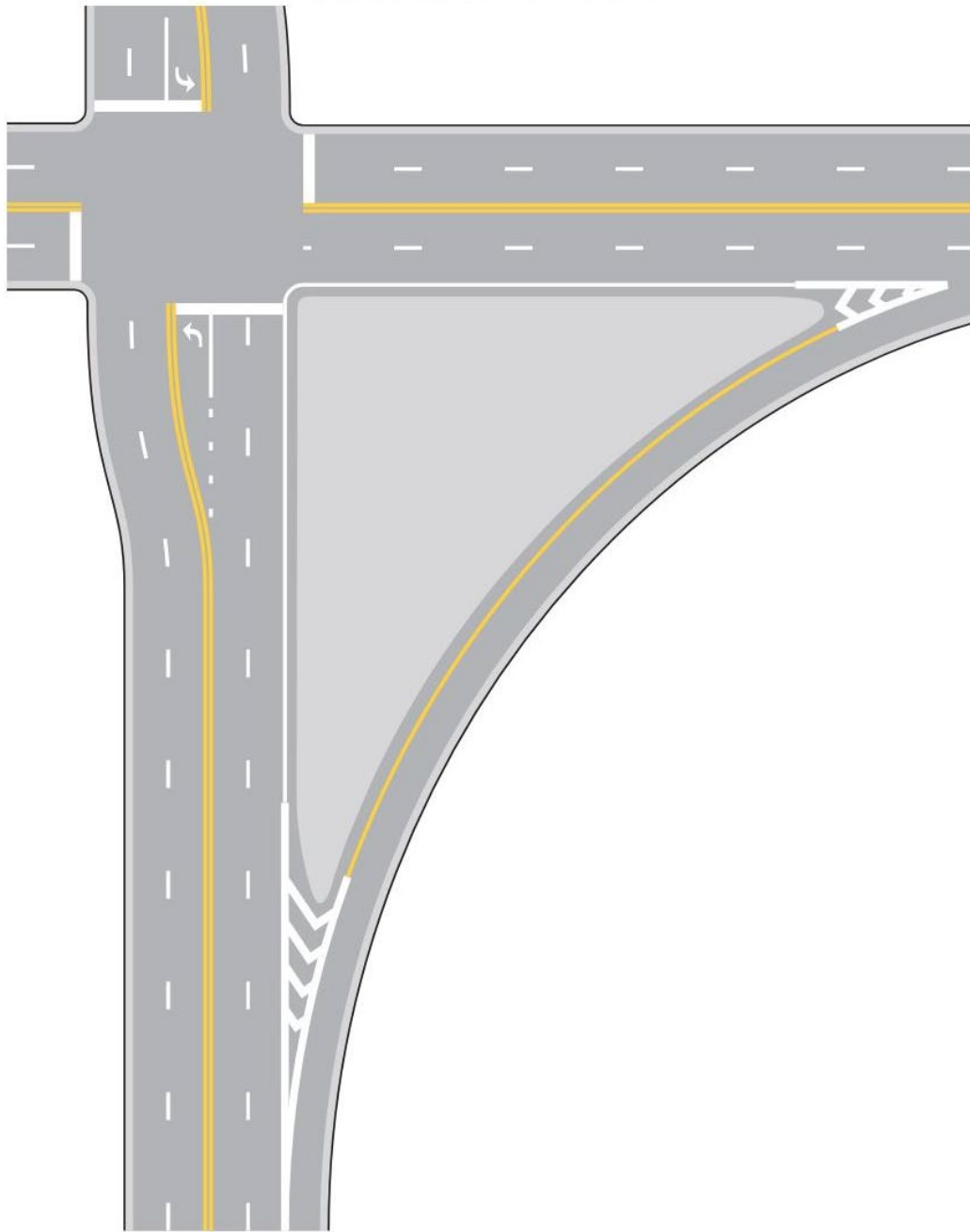
A. White Channelizing Lines Applied to a Raised Island
Separating Traffic Flow in the Same General Direction



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Figure 3J-4. Example of Pavement Markings for Raised Islands
(Sheet 2 of 2)

B. Yellow Edge Line Applied to Raised Island Separating Traffic Flows in the Same General Direction

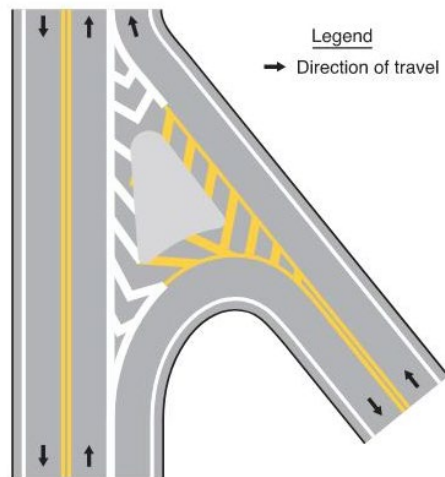


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162 **Figure 3J-5 Comments: NCUTCD agrees with Figure 3J-5 as presented in the NPA.**

163 **Figure 3J-5. Example of Pavement Markings Contiguous to a Raised Island**

Figure 3J-5. Example of Pavement Markings Contiguous to a Raised Island



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166 **Section 3J.06 Comments: NCUTCD agrees with 3J.06 as presented in the NPA.**

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168 **Section 3J.06 Island Delineation**

169 **Standard:**

170 **Delineators installed on islands shall be the same colors as the related channelizing or**
171 **edge lines except that, when facing wrong-way traffic, they shall be red (see Section 3G-3).**

172 **Each roadway through an intersection shall be considered separately in positioning**
173 **delineators to assure maximum effectiveness.**

174 **Option:**

175 **Retroreflective or internally illuminated raised pavement markers of the appropriate color**
176 **may be placed on the pavement in front of the curb and/or on the top of curbed approach ends of**
177 **raised medians and curbs of islands, as a supplement to or as a substitute for retroreflective curb**
178 **markings.**

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181 **Section 3J.07 Comments: NCUTCD agrees with 3J.07 as presented in the NPA.**

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183 **Section 3J.07 Curb Extensions Designated by Pavement Markings**

184 **Support:**

185 Curb extensions are used to extend the sidewalk or other pedestrian space, shorten crossing
186 distances for pedestrians, alter the roadway geometry for speed control or channelizing, and for
187 other purposes.

188 Curb extensions are typically created by physical infrastructure including concrete or asphalt,
189 but can also be designated by pavement markings.

190 For the purposes of this Section, the paved areas between the solid double line forming the
191 curb extension (see Paragraph 4 of this Section) and the sidewalk or other roadside area are
192 considered to be outside of the street.

193 **Standard:**

194 **Curb extensions formed by pavement markings shall be established using solid double**
195 **lines. The color of the solid double line shall comply with the general principles of**
196 **markings (see Section 3A.03).**

197 *Guidance:*

198 *Physical separation or delineation should be provided where curb extensions are created by*
199 *pavement markings and pedestrian travel can be expected within the area created by the curb*
200 *extension.*

201 *Option:*

202 Channelizing devices such as tubular markers (see Chapter 3I) may be used along the solid
203 double line of a curb extension created by pavement markings.

204 Other methods of physical separation may be used beyond the solid double line outside of the
205 limits of the street.

206 *Guidance:*

207 *Diagonal markings (see Section 3B.24) or colored pavement (see Chapter 3H) should be*
208 *used within the marked curb extension to emphasize that the area is outside of the street.*

209 *Support:*

210 Curb extensions are distinct from areas within the street such as shoulders, flush medians, or
211 gore areas where travel is discouraged by the presence of diagonal markings (see Section 3B.24).
212 Curb extensions designate areas outside of the street where travel is prohibited.

213 *Guidance:*

214 *Where pedestrian travel can be expected within curb extensions created by pavement*
215 *markings, adequate provisions should be made for pedestrians with disabilities.*

216 *Support:*

217 Additional information on the design and construction of accessible facilities is found in
218 publications listed in Section 1A.05 (see Publications 12 and 42).

219 Figure 3J-6 illustrates examples of curb extensions designated by pavement markings.

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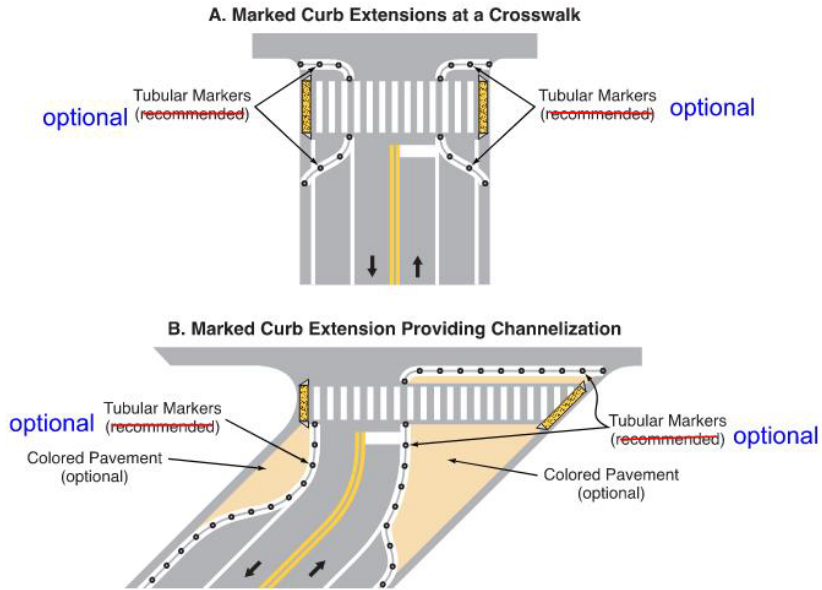
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223 **Figure 3J-6 Comments:** NCUTCD generally agrees with Figure 3J-6 as presented in the NPA,
224 but recommends replacing “recommended” in the callouts with “optional”.

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Figure 3J-6. Examples of Curb Extensions Designated by Pavement Markings

Figure 3J-6. Examples of Curb Extensions Designated By Pavement Markings



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